

Gainsborough Bridge Wayfinding Strategy

Hackney Wick

Discharge of Planning Conditions- Wayfinding Strategy (GB.7) and Wayfinding Details: Waterways (GB.8)

This document has been prepared to satisfy two planning conditions related to the delivery of a Wayfinding Strategy (Planning condition GB.7) and submission of Wayfinding Details: Waterways (Planning condition GB.8) for LLDC Planning application reference 12/00261/FUL:

Planning application reference 12/00261/FUL: Condition GB.7 Wayfinding Strategy

Prior to the bridge being opened for public access a wayfinding strategy shall be submitted to the Local Planning Authority for approval. The wayfinding strategy shall have been prepared, in consultation with the London Boroughs of Hackney, Newham, Tower Hamlets and Waltham Forest, the Lee Valley Regional Park Authority, Transport for London, the Canal and River Trust and the Built Environment Access Panel. The strategy shall have regard to the London Legacy Development Corporation Inclusive Design Standards. It shall contain details of wayfinding for the bridge and how that wayfinding relates and integrates appropriately with the wayfinding proposals and strategy for the Stratford City Development and the Greenway and shall contain a timeframe for implementation. All measures contained within the approved wayfinding strategy shall be provided in accordance with the approved details and within the approved timeframe (unless alternative phasing has been agreed by the Local Planning Authority in writing) and retained as approved thereafter throughout the life of the bridge hereby approved.

Reason: To ensure a high level of legibility and access throughout the Park in accordance with policies 7.1 and 7.5 of the London Plan and policy 26 of the London Borough of Hackney Core Strategy.

Planning application reference 12/00261/FUL: Condition GB.8 Wayfinding details: Waterways

Prior to the opening of the bridge, details of a scheme to aid navigation and wayfinding for users of the River Lee Navigation (Waterways Wayfinding Strategy) shall be submitted to the Local Planning Authority for approval. The details shall include how colour, design or signage will be used to aid wayfinding and navigation from the water and along tow paths. All measures contained within the Waterways Wayfinding Strategy approved pursuant to this condition shall be provided in accordance with the approved details prior to 31st December 2013 (unless alternative phasing has been agreed by the Local Planning Authority in writing) and retained thereafter throughout the life of the bridge hereby approved unless minor variations are agreed in writing by the Local Planning Authority. “

Reason: To ensure a high level of legibility along waterways and towpaths throughout the Park in accordance with policy 7.27 of the London Plan and policy 28 of the London Borough of Hackney Core Strategy.

1. Introduction

1.1 Gainsborough Bridge passes over the Lee Navigation canal between Wick Village estate and Gainsborough School, and Here East on the Queen Elizabeth Olympic Park (QEOP) (see figure 1 and figure 2 below). In 2014 the current Gainsborough Bridge was built to replace a previous bridge in this location. The bridge is owned by Hackney Council and the Planning Authority for the area in which the bridge is located is the London Legacy Development Corporation (LLDC).

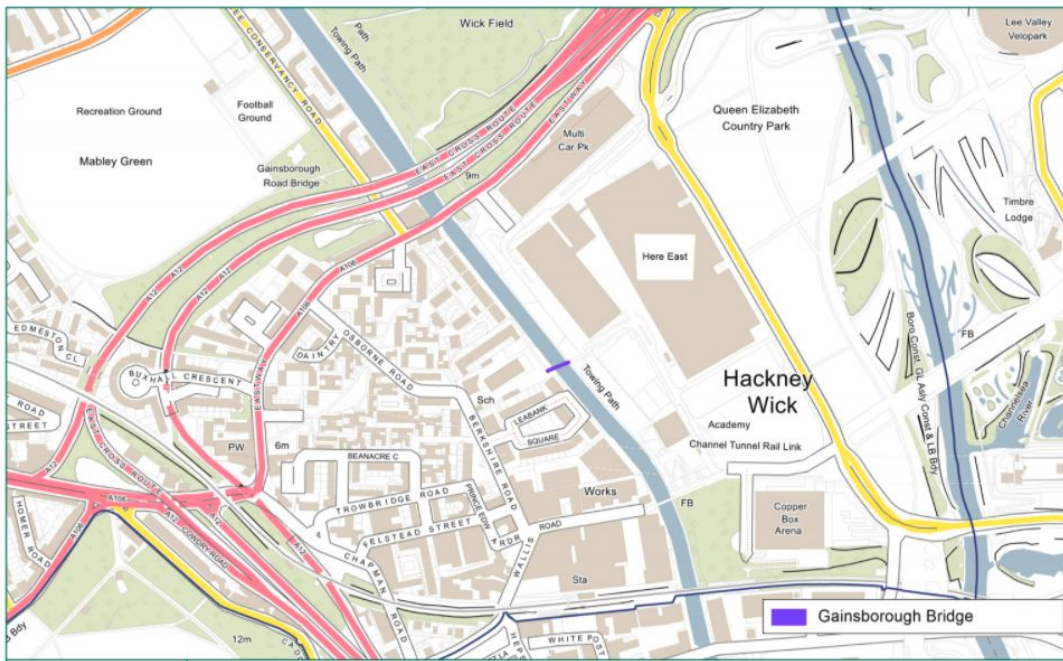


Figure 1 - Location of Gainsborough Bridge

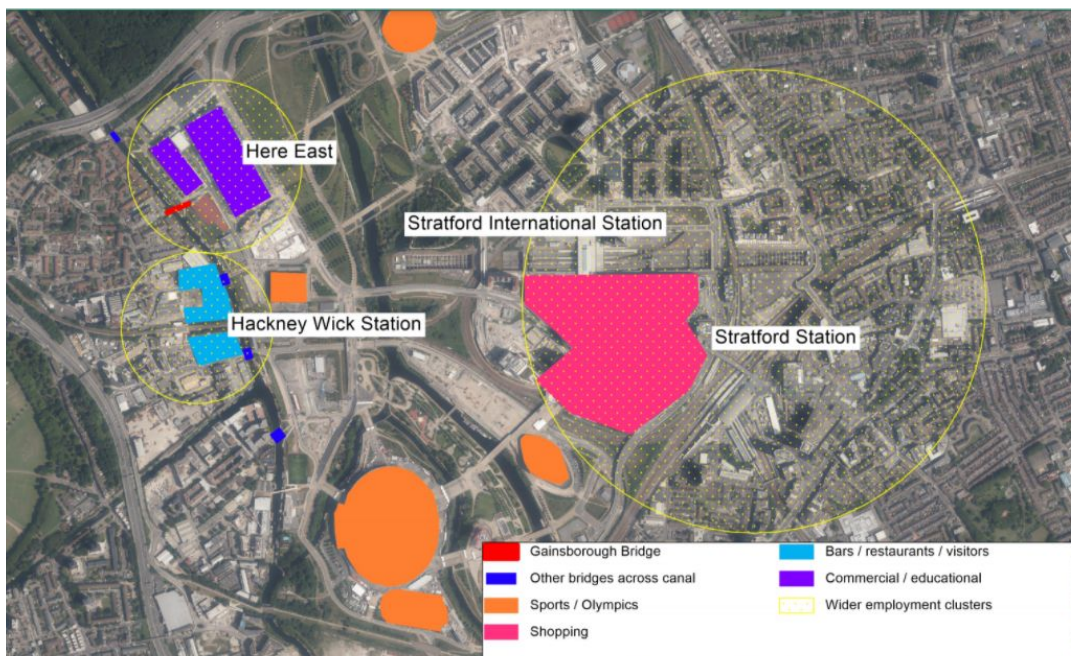


Figure 2 - Location of Gainsborough Bridge

- 1.2 The LLDC granted planning permission for Gainsborough Bridge in 2013 and attached a number of planning conditions to the permission, including the requirements to produce and submit for approval to the LLDC both a Wayfinding Strategy and Wayfinding Details for the Waterways (in addition to a Public Access Strategy), before the bridge could be opened to the public.
- 1.3 The details of the planning conditions relating to the Wayfinding Strategy and Wayfinding Details: Waterways is set out above. The Gainsborough Bridge planning decision notice issued by LLDC (planning application reference 12/00261/FUL) and associated planning documents can be viewed here: bit.ly/2EOnmHi.
- 1.4 Pre-application advice from the LLDC stipulated that the Waterways Wayfinding Strategy could be included as part of a wider Wayfinding Strategy, therefore both planning conditions outlined above have been combined in this document as a single Wayfinding Strategy for Gainsborough Bridge. This Wayfinding Strategy aligns with the London Plan and the LLDC Local Plan with regard to improving connectivity, access and cycling.
- 1.5 A key design principle of the QEOP is to provide regular connections - via footpaths, bridges and crossings - to promote ease of access to and across the Park, and to the areas directly outside of the Park, with good intuitive navigability (LLDC Park Design Guide 2018). Hackney Council believes Gainsborough Bridge is a key connection to the QEOP and should be accessible to all members of the public.
- 1.6 The opening of Gainsborough Bridge is part of the long term aspiration to improve pedestrian and cycle routes in the Hackney Wick area. It is intended to reduce the severance that the canal creates between Hackney Wick and the QEOP, encouraging sustainable transport to and from the park as opposed to people using private motor vehicles.

2. Proposed Wayfinding Strategy for Gainsborough Bridge

- 2.1 In order to open Gainsborough Bridge for public access, a series of public realm improvements are proposed. Considering the location of the bridge, between two more direct connections across the canal at Eastway and Wallis Road, its public use is likely to improve connections for local people rather than provide a primary route or shortcut for people travelling through the area from other destinations. Therefore, the proposed improvements do not specifically for first time users to the area but are more local in their focus.
- 2.2 Proposed public realm improvements as part of the wayfinding strategy include:
 - New signage on both sides of the canal to improve wayfinding

- Tree planting, if feasible
- Removing gates from each side of the bridge

The proposed public realm improvements will enhance the experience of local users of the bridge, with a focus on catering for children travelling to school, parents with prams, elderly people and people with physical disabilities who may benefit from a safe and accessible connection across the canal.

2.3 Although opening Gainsborough Bridge for public use is likely to primarily improve local connectivity, Hackney Council will also ensure this route enables accessibility for local residents, businesses and visitors, by implementing wayfinding initiatives in the immediate area. These initiatives will signpost potential users to the bridge and will be implemented prior to it being opened to the public. Hackney Council is also developing, in consultation with local stakeholders, a wider wayfinding programme to update or install new signage on the public highway and on the canal towpath in Hackney Wick, subject to feasibility, funding and agreement from TfL and the Canal and River Trust.

2.4 Hackney Council’s Traffic Engineers have identified three locations where new signposts to the bridge could be located to improve local wayfinding in the immediate area:

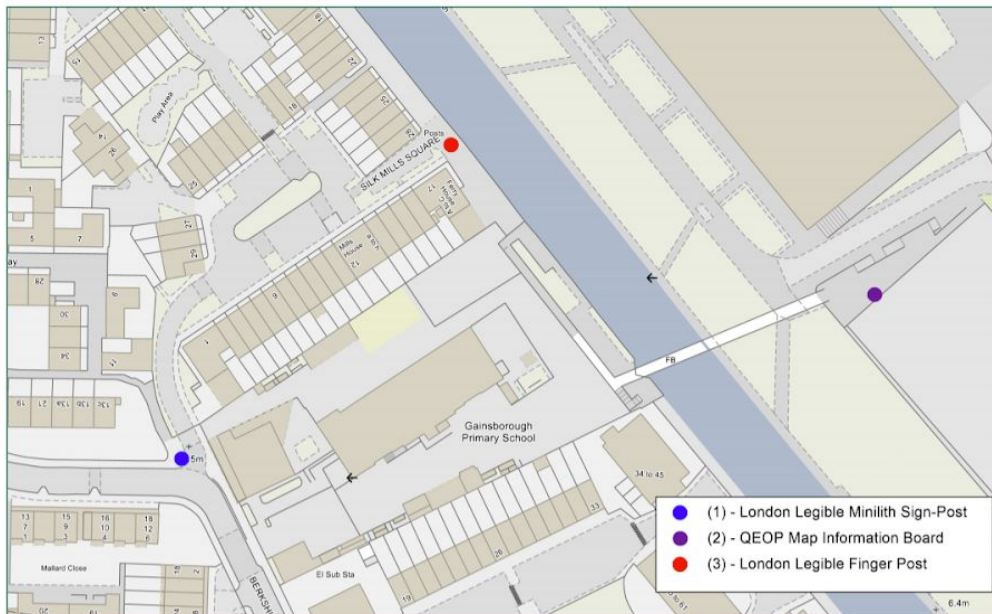


Figure 3 - location of proposed new wayfinding signage

2.5 Pending relevant permissions, Hackney Council will install two signposts on Council land prior to Gainsborough Bridge opening to the public:

2.5.1 *Signpost point (1) - London Legible Minilith Sign-Post* - This point is a clearly defined connective link to the Hackney Wick neighbourhood centre and other key community assets. Hackney Council will fund the installation and will work closely with Transport for London (TfL) to deliver this signpost.



Figure 4 - Proposed location of *Legible London Minilith Sign-Post* and example image of *Legible London Minilith Sign-Post*

2.5.2 *Signpost point (2) - QEOP Map Information Board* - This point is a gateway point for users entering into the QEOP. A Map Information Board will identify key landmarks in the Park such as the Copper Box, or Greenway; LLDC have agreed to provide, and install a QEOP Map Based Information Board at this location at no cost to Hackney Council.



Figure 5 - Proposed location of QEOP Map Information Board and example image of QEOP Map Information Board

2.6 Pending further consultation with the Wick Village Tenant Management Co-operative Limited (TMC) and on obtaining all relevant permissions, Hackney Council could also install a third signpost next to the canal:

2.6.1 *Signpost point (3) - London Legible Finger Post* - This point is at the defined T junction of Silk Mills Square and Windsor Wharf. Footfall will be either directed to the Hackney Wick station, Eastway or QEOP.

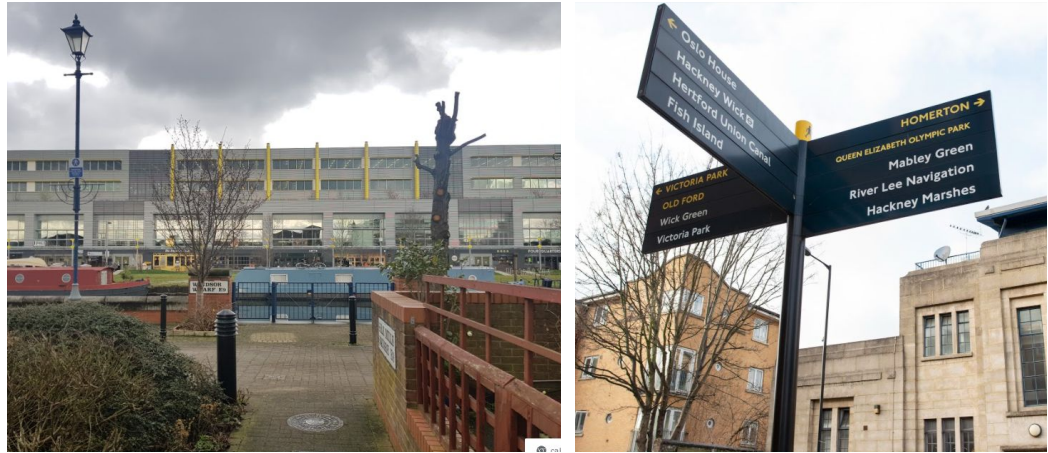


Figure 6 - Proposed location of *Legible London Finger Post* and example image of a *Legible London Finger Post*

2.7 All new signage will be in keeping with the LLDC's QEOP Wayfinding Strategy (2013) and Inclusive Design Standards document (2019). Current wayfinding to Gainsborough Bridge from the west entrance is difficult and there are minimal spatial, physical or environmental clues to help people plan and navigate moving from one place to another. By using London Legible and QEOP signage, the new wayfinding signage will provide a simple consistent method for people to find their way to the bridge and to other locations in the local area from the bridge. The Wayfinding Strategy for Stratford City development and Greenway already integrates a number of individual signage schemes, including Legible London and QEOP signage. Therefore Hackney's use of Legible London and QEOP signage will also ensure that the additional wayfinding surrounding Gainsborough Bridge is integrated appropriately with these strategies, and create consistency across the QEOP and surrounding neighbourhoods.

2.8 Hackney Council will position each of the proposed signposts to ensure footway width standards are maintained. Once a decision has been made by the LLDC regarding the opening of Gainsborough Bridge for public use, Hackney Council's Transport department will develop detailed designs for the new signage in consultation with local stakeholders before implementing the new signage.

3. Implementation Timeframe

3.1 If the Public Access Strategy, Wayfinding Strategy and Wayfinding details: Waterways planning conditions are discharged by the LLDC, Hackney Council will commence detailed design work for the proposed signage and implement the proposed signage within six months of the approval. Once installed, Hackney

Council will remove the two gates at each end of Gainsborough Bridge enabling unrestricted public access to the bridge.

4. Consultation Responses to the draft Wayfinding Strategy

4.1 A draft Wayfinding Strategy was shared with the London Boroughs of Newham, Tower Hamlets and Waltham Forest, the Lee Valley Regional Park Authority, Transport for London, the Canal and River Trust and the LLDC Built Environment Access Panel for comment which is a requirement of the *Wayfinding Strategy (GB.7)* planning condition. Responses were received by the LLDC's Built Environment and Access Panel, TfL and the Canal and River Trust. Below is a summary of the comments received and details of Hackney Council's response and inclusion in the final Wayfinding Strategy for Gainsborough Bridge:

Commenting Body	Comment	Response/Mitigation
LLDC Built Environment and Access Panel	It is felt that even though the final signage may comply with the relevant standards that it might not be sufficient to give first time users the confidence to access the bridge through the residential area	Most primary routes encourage people to cross the canal on the two bridges either side of Gainsborough Bridge, walking along the Eastway or Wallis Road. Bridge users are therefore unlikely to use Gainsborough Bridge if visiting the area for the first time and are more likely to use the Wallis Road and Eastway bridges to cross the Lee Navigation canal. Separate to the Gainsborough Bridge Wayfinding Strategy, Hackney Council is considering additional wayfinding initiatives in the wider area to encourage accessibility to local residents, businesses and visitors.
LLDC Built Environment and Access Panel	Concern that the Minilith Signpost proposed might not be sufficient or in the right location.	Initial consultation between Hackney Traffic Engineers and TfL has confirmed that a minilith signpost is appropriate for this location.
LLDC Built Environment and Access Panel	The Panel require details of the design of the Minilith Signpost, Finger post and information board in addition to the proposed location in order to comment sufficiently	If the LLDC discharge the conditions relating to Gainsborough Bridge, Hackney Council will develop detailed designs for the new <i>Legible London Minilith Signpost</i> and <i>Legible London Finger Post</i> signage in consultation with local stakeholders. The Map

		Information Board will be provided and installed by the LLDC, at no cost to Hackney Council.
LLDC Built Environment and Access Panel	Further clarity and detailed description including drawings (or photos) of the exact location of new signposts to the bridge is also required to ensure people with different access needs can navigate successfully.	The final Gainsborough Bridge Wayfinding Strategy has been amended to reflect this comment.
Transport for London (TfL)	(Point 2.5) Funding and delivery: Would the signposts be funded by LBH to ensure the scheme is delivered? Worth making clear at some point.	The Council has agreed to fund the new <i>Legible London Minilith Signpost</i> and <i>Legible London Finger Post</i> signage required for this project. The information board will be provided and installed by the LLDC.
Transport for London (TfL)	(Point 2.5.1) Suitable location / decision point for a LL minilith	Comment noted.
Transport for London (TfL)	(Point 2.7) Strictly speaking Stratford City has a mix of individual schemes (see East Village) as well as some LL signs.	The final Gainsborough Bridge Wayfinding Strategy has been amended to reflect this comment.
Transport for London (TfL)	(Point 2.7) Would need to establish when any refresh is done and / or find a way to fund this.	The Council intends to complete any updates to the area's existing infrastructure in tandem with the installation of new signage, subject to sourcing additional funding. Additional funding will be sought from available S106 and CIL funding.
Canal River Trust	We agree with the locations suggested for additional wayfinding.	Comment noted.
Canal River Trust	The wayfinding strategy should also include updating the two QEOP finger posts that point from the towpath towards Here	Hackney Council will consider these locations as part of their wider updates to wayfinding in the area.

	East and the bridge, to promote the route over the canal for people on the towpath, photos attached.	
Canal River Trust	<p>We would also like the map panels of the two nearest Legible London minilith map signs to be updated to show the new bridge crossing, as identified in the locations below:</p> <ol style="list-style-type: none"> 1. The minilith on the towpath just south of the A106 Eastway, which has had the map panels vandalised since this photo. 2. The minilith on the towpath just south of White Post Lane. 	<p>The Council will consider the first location as part of their wider updates to wayfinding in the area. The second location is in Tower Hamlets and therefore Hackney Council will pass this information on to the relevant officers within Tower Hamlets Council.</p>
Canal River Trust	<p>You should also be aware that you will need to sign up to the Canal & River Trust Code of Practice for works on or close to our property. Please refer to the Code of Practice here:</p> <p>https://canalrivertrust.org.uk/business-and-trade/undertaking-works-on-our-property-and-our-code-of-practice</p>	<p>Comment noted.</p>

- 4.2 Following receipt of the above comments, Hackney Council amended the final Gainsborough Bridge Wayfinding Strategy to address the comments received:
- Hackney Council has confirmed that funding has been secured to deliver the *Legible London Minilith Signpost* and *Legible London Finger Post* signage and that the LLDC will fund the Map Information Board.
 - Hackney Council has confirmed that there will be a wider wayfinding review completed in Hackney Wick which will consider updating existing signage in consultation with local stakeholders if feasible and if funding can be secured.
 - Hackney Council has included images of the proposed style of signage within the final Gainsborough Bridge Wayfinding Strategy and has now confirmed that the detailed designs of the proposed signage will be completed in conjunction with local stakeholders once a

decision has been made regarding the opening of Gainsborough Bridge to the public.

5. Summary

5.1 This Wayfinding Strategy has been prepared in consultation with the LLDC's Built Environment and Access Panel, TfL and the Canal and River Trust. The Wayfinding Strategy was also shared with London Boroughs of Newham, Tower Hamlets and Waltham Forest, and the Lee Valley Regional Park Authority, however no comments were received. Pre-application advice from the LLDC to combine the Waterways Wayfinding Strategy with the Wayfinding Strategy for Gainsborough Bridge has been completed and an approach has been taken that integrates with local wayfinding proposals, existing strategies and design standards. The LLDC Inclusive Design Standards are adhered to and the Gainsborough Bridge Wayfinding Strategy integrates appropriately with the wayfinding proposals and strategy for the Stratford City Development and the Greenway. Hackney Council has secured the funding to deliver the *Legible London Minilith Sign-Post* and *Legible London Finger Post* signage and has the resources to deliver the scheme within six months of the planning condition being discharged. Lastly, Hackney Council will work with TfL and Canal and River Trust to update *Legible London* signage located within Hackney Wick and along the canal towpath where feasible and if funding is secured for the works.

